

ATHS NUTMEG CHAPTER STEERING COLUMN

MARCH-APRIL 2021 – OUR THIRTY- EIGHTH YEAR

DON ARNAUCKAS, EDITOR
14 AVALON AVENUE
OAKVILLE, CT 06779

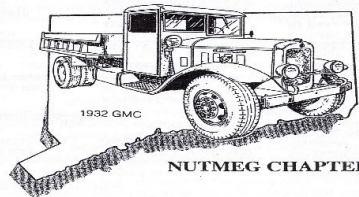
MARCH MEETING

Cancelled due to Covid-19

Restrictions

Stay Well*Stay Safe*Stay in Touch

American Truck Historical Society



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"Dedicated to preserving the dynamic history of trucks, the trucking industry, and its pioneers."

PRESIDENT'S MESSAGE

March-April 2021

Unfortunately, due to the continuance of the Connecticut State of Emergency orders and in order to keep our members safe, we will not have our March meeting. We will schedule a meeting as soon as we can after the State gives the green light for social gatherings that can safely accommodate more than 25 people. With spring around the corner (we hope), perhaps an outdoor meeting would be feasible. Our next meeting will also be our Annual Meeting with Election of Officers and Board of Directors. While our 2021 Truck Show continues in limbo until we get the word from the Brooklyn Fairgrounds about the status of events for 2021, we will be listing our tentative truck show in Wheels of Time and Double Clutch like we did last year. If the shows are cancelled, we can remove our show listing from those websites. A number of other truck clubs are doing the same thing. We can be optimistic and hope for the best in the months to come.

I want to thank our members who updated their contact information when they paid their annual dues. Lance Dimock and Tom Bachand have compiled a new roster with all that up-to-date information. This will be helpful in keeping in contact with our members. We do not share your information and all communications are sent as 'blind copy'.

In our latest issue of Wheels of Time, they listed the members that have received awards in 2020. Most likely, there are some of our Chapter members who are qualified for these awards. This would be a good time to give some thought to our members who could be nominated to receive an award in 2021. On the ATHS web site, they list the qualifications and you will find the applications for nominating someone. If you do not have access to the web site, I will be happy to print this information out and mail it to you. Just let me know and I will get it to you ASAP.

With many of us shut in because of the cold weather and the fear of the Covid-19, this is be a good time to write something up to contribute to **our** newsletter and the Wheels of Time. Members would be interested in your restoration work, some old road or work stories, or you could dig out some old photos of trucks that you drove or owned (or maybe still own). With no truck shows last year and very few meetings, that is all the less news and items to include in our newsletter. The Newsletter is about our members, so feel free to contribute to it. If there is a certain truck model you would like to see featured let me know. Possibly, I might have that in my collection. In this Newsletter, I picked out some information on electric trucks, both old and new, Chevy and Diamond T pickups, the favorite Mack Super Liner and the inner workings of a LTL trucking company. I hope there was something for everyone.

It is with sadness that we learned of the passing of Nutmeg member Don Smith of New Hampshire. Don and Marilyn have attended our Brooklyn Show many times. Don would always give us a hand and Marilyn would be taking pictures that she would share with us. Don was active in the ATHS Granite State Chapter and later as President of the ATCA Granite State Chapter.

We look forward to being together in the very near future. In the meantime, Stay Safe, Stay Well, Stay Sane and Keep Warm.

Don Arnauckas, President

This holiday greeting arrived too late to publish in our Jan-Feb Newsletter. Thank you Maggie for your message.

Christmas 2020

So this is Christmas, the
most beautiful time of the
year.
With songs of the season to
focus us on believing.
To me, 2020 is perfect vision.
But in March we were hit with
a perfect storm (Covid-19).
They say it came from China.
It spread faster than a wild
fire without water.
It has wreaked havoc from
border to border.
Most disasters are area bound,
but this one is worldwide found.

It has touched the good, the
bad and vulnerable. And turned
our lives upside down.
We have all been born to this
physical place from a spiritual
realm.
And by pulling together in Love,
Wisdom, Strength, Compassion, and
Doing Covid will be disarmed.
Just maybe the Baby Jesus will
appear and give us the all clear.

We can only hope.
Love and Best Wishes for the Holidays &
New Year! (stay safe)

Maggie McCormick

2021 TENTATIVE CALENDAR OF EVENTS

Please Note: AS of Feb. 9 these dates are tentative and listed for informational purposes only!!!

- TBA** **ATHS Nutmeg Chapter** will schedule a meeting (**hopefully in May 2021**) once we can meet safely under the Covid-19 guidelines for social gatherings. Watch for details on our Website, Facebook pages, the Newsletter and email to those we have email addresses for.
- June 3-5** **The ATHS National Convention and Truck Show** is scheduled to be held in Harrisonburg, Virginia on **June 3-5, 2021**. Consult your Wheels of Time or ATHS.org for details.
- June 12 & 13** **ATCA CT Yankee Chapter Annual Show** at the Bethlehem Fairgrounds Route 61 Bethlehem, CT is tentatively scheduled for June 12 & 13. Contact John Famigiletti at 860-307-4372 for more info.
- June 18 & 19** **ATCA 41st Annual National Meet in Macungie** at Macungie Memorial Park, Route 100, Macungie, PA is tentatively scheduled. Contact Tom Oehme at 717-626-1204 or email him at thomocc@yahoo.com.
- JUNE 27** **ATHS Nutmeg Chapter Brooklyn Truck Show is tentatively scheduled for June 27, 2021** and subject to State of CT and Brooklyn Fairgrounds determinations. We will keep you updated as whether or not we can hold our show. Details will be made available when determination is made.
- July 18** **The Uncle Sam Chapter (ATCA) Antique Truck Show** is tentatively scheduled for July 18, 2021 at the Washington County Fair, Rte 29, Greenwich, NY. Contact Al "Raz" Macauley at 518-677-8264 or Clarence Ritchie at 518-642-9437 for info.
- July 30 & 31** **4th Annual Gear Jammer Truck Show** at the Monadnock Speedway in Winchester, NH is tentatively scheduled for July 30 & 31, 2021. Contact Robert Conrad at 508-212-9998 or email mttconrad7@aol.com for more info.

Donald Manton Smith January 27, 1934 – December 22, 2020

The Nutmeg Chapter extends our sympathy to the family of long-time Nutmeg member Donald Manton Smith, 86 who died unexpectedly on December 22, 2020. He was the husband of Marilyn I. (Wilbur) Smith. They had shared sixty-eight loving years of marriage. Don and Marilyn attended many of our truck shows over the years, lending a helping hand and taking photos. He was the only child of the late Winthrop L. and Helen (Farrow) Smith. As a youngster, he was a member of the Boy Scouts. A 1952 graduate of Oliver Ames High School, he went on to serve in the National Guard from 1952 to 1960. He was also volunteer fireman in Easton. Self-employed his entire life, he installed driveways and operated a motorcycle shop as well as plowing for the state of Massachusetts. In 1972 he moved his family and business, D.M. Smith Excavating to Barrington, NH. He also plowed snow for the state of New Hampshire and many clients.

His affinity for antique cars and motorcycles led him to become a member of numerous organizations including the American Truck Historical Society, Nutmeg Chapter ATHS, Antique Truck Club of America-Granite State Chapter, the Studebaker Club, and International Tractors. He was also a member of the American Legion.

In addition to his wife Marilyn, he is survived by his children. Evelyn, wife of Ron Musgrove, Eugene, husband of Rose Smith, Kenneth, husband of Kathy Smith, Donna, wife of Darryl Riley; a daughter-in-law, Shawna Smith, nine grandchildren, and twelve great-grandchildren. He was the father of the late Lawrence "Larry" James Smith, who passed away unexpectedly on September 3, 2020.

Donations may be made in Donald's name to: American Legion, Post 114, 731 Franklin Pierce Highway, Barrington, NH 03825. Funeral services were held in Lee, NH.

Slate of Officers for 2021 & 2022

PRESIDENT: Charlie Dainton
VICE PRESIDENT: Harold Foskett
TREASURER: Tom Bachand
SECRETARY: Betty Jay
AT-LARGE: Peter Yeomans
AT-LARGE: Ken Anderson
AT-LARGE: Denis Yaworski
AT-LARGE: Calvin Neeley

Note : Membership, Newsletter editor, Show Chair & Web Masters are appointed positions.

HAVE WE COME A LONG WAY ??????????

By: Don Arnauckas

In our modern age of on-line shopping and next-day delivery (speedy delivery) this is just our new way of living. This past Christmas everyone had stories of to tell of those 'must have' items that they ordered and how long it took to get it. Because of the high volume, the delivery services were maxed out. In fact some of us are still waiting for that one item that never showed up under the tree!

My memories go back to the early 1980's when I was a road driver for Lombard Bros. of Waterbury, CT. As a less-than-truckload common carrier, they offered "LTL Consolidation and Line Haul" service. As an example, Bata Shoe Co. in Belcamp, MD used Lombard for all their small shipments of raw materials coming into their factory in Belcamp, MD. All their suppliers from Maine, south to Maryland would use Lombard Bros. Bata Shoe Co., known as the "military boot maker" was the biggest factory in Belcamp, MD at that time.

Soon after one started driving for Lombard Bros., you learned that Bata Shoe was a "Priority Customer". The management and the other drivers would fill you in on how to handle the Bata shipments. Their system of handling this account was unique in the early 1980's.

Local drivers who picked up a Bata shipment would always keep it at the rear of their trailer, and to the side. When they returned to their terminal at the end of the day, that Bata freight would go on to a load of miscellaneous freight, always on the rear, to be the first off at the next terminal. As an example a shipment out of Portland, Maine would go to Hartford, Connecticut, where it would meet up with an inbound line-haul from Boston and Worcester, Massachusetts. The dock crew worked all night and they knew the Bata freight was always on the rear of the trailer. In no time, that Bata freight was off to Bridgeport, Connecticut where the line-haul from Providence, Rhode Island and Waterbury, was loaded on to the truck. The next stop might be at the Philadelphia, Pennsylvania terminal where the inbound from Elizabeth, New Jersey and Allentown, Pennsylvania would be added on to the trailer. The final (next) stop would be at Bata Shoe in Belcamp, MD, early the next morning, when the dock crew were starting their days' work. The trailer most likely was a full load of small shipments from all their suppliers from the Northeast. These shipments had been picked up at the end of the workday at various shippers and were at their destination the next morning. This was before "Just-In-Time" deliveries.

It was also before all the electronics of today, computers, satellite tracking, smart phones, barcode scanning and electronic logs. The newest break through at this time was the WATTS 1800 number, so you didn't have to carry a pocket full of change to call the terminal from a pay phone. The trucks were equipped with a 250-290hp diesel and a 10 or 13 speed manual transmission and spring ride. They had no power steering, no air conditioning, no air ride seats, and no radios. Most of the drivers never went to Truck Driver School and many didn't finish High School. Many were born in other countries, learned our language and came here seeking a better life. These drivers learned quickly that being a truck driver was a better than average blue-collar job. They learned that when the Lombard dispatcher said you had Bata Shoe on your trailer, you weren't stopping at the "Green Comet" to fall in love with the waitress and tell road stories with the other drivers. The work ethics and the dedication of the management, dispatchers and drivers made this system work. Overnight service of small shipments going from Maine to Maryland worked well in 1980 and is still remarkable even in 2021 !!!!!



Lombard's main terminal was located at Exit 25 I-84 on Reidville Drive in Waterbury, CT. Lombard Plaza occupies the area today with Burger King on the right and to the left is CVS Pharmacy (old loading docks). To the rear is Aldi's which once housed the maintenance garages.



1970's White Road Boss Cummins 290 13 SP



1970's White 9000 (Refurbished) Cummins 250 10 SP



1975 White Road Boss (Glider) Cummins 290 13 SP



1976 Ford LN-9000 6V92TT Detroit Diesel



1979 International S-2275 with a 45 Ft FRP Trailer

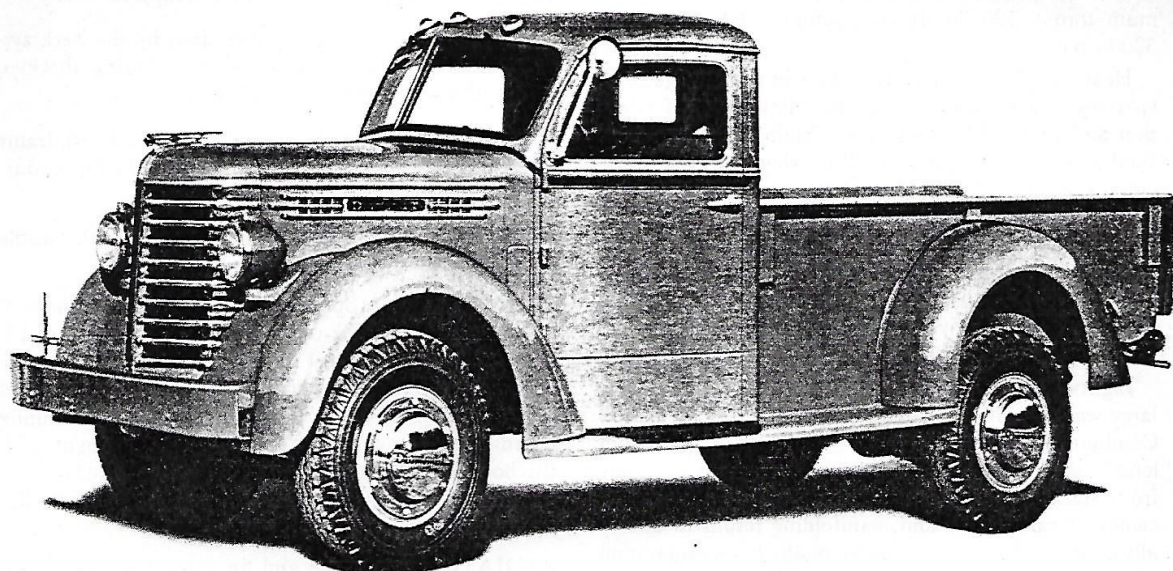


1978 International S-2275 Cummins F-250 10 SP



1976 White 6000 with a V-8 CAT Diesel 10 SP

DIAMOND T MODEL 201



Nominal Capacity 1 Ton

Maximum Gross Weight 8,000 lbs.

CHASSIS SPECIFICATIONS

AXLE, FRONT: Extra heavy drop forged I-beam, over-size king pins. Roller bearings at spindles.

AXLE, REAR: Spiral bevel, exceptionally rugged construction, full-floating design with double Timken wheel bearings directly above tires. Axle shafts carry no load and are removable without taking off wheels. Pinion straddle mounted on three rows of bearings. One-piece forged housing of heat-treated steel. All gears and shafts of heat-treated alloy steel. Removable inspection plate. Axle ratio 4.62 to 1.

BRAKING SYSTEM: Latest type Lockheed internal expanding hydraulic brakes on all four wheels. These brakes provide superior stopping ability together with long life in severe service. They require no lubrication and operate identically whether truck is empty or loaded. Braking pressure is automatically equalized. Heavy cast brake drums of fine grained alloy iron, 14" in diameter and 2" in width. Lining area 210 sq. in. Finest quality heavy duty molded lining. Hand brake is a contracting band mounted at the rear of the transmission.

CAB: (Special Equipment) The new Diamond T Vee-type safety steel cab of unequalled distinction of appearance. Especially roomy and comfortable with an overall width of 60 inches. One-piece steel roof, sound and heat insulated. Streamline Vee-type twin windshields, individually controlled by separate cranks. All hardware of exceptional quality including three heavy hinges for each door, two windshield wipers, rear view mirror, sun-visor, dome-light, glove compartment, rubber floor mat. Door

windows crank operated. Safety glass in windshield and all windows. Durable and comfortable air-spring cushions of textile leather are of eight inches depth. Large built-in steel tool compartment with lock. Cab finished in oven-baked enamel, red and black.

CARBURETOR: Latest type down-draft.

CLUTCH: Single plate of unusually large capacity, 10" in diameter. Flexible mounting of driven plate provides smoother engagement and freedom from chatter. Self-lubricated ball throw-out bearing.

EQUIPMENT: Streamline radiator shell with stainless steel grill and moldings. Heavy gauge full coverage front fenders. Chrome-plated hood ventilating grilles and radiator ornament. Deep flanged running boards with non-slip inserts. Sealed beam head lamps with foot control and parking lights. Double-acting hydraulic shock absorbers front.

Bumper	Speedometer	Tail light
Battery	Oil-bath Air-Cleaner	Stop light
Generator	Gas and Oil Gauges	Spare rim
Starter	Temperature Indicator	Horn
Ammeter	Cowl and Ventilators	Jack and Tools

ENGINE: The Diamond T heavy-duty six, engineered with major Super-Service features to give exceptionally long life and maximum economy. L-head engine design with the cylinder block and upper crankcase cast in one piece of electric furnace alloy iron, heat-treated.

DIAMOND T 1-TON MODEL 201

Bore and stroke are $3\frac{7}{8}$ " x $4\frac{1}{4}$ ", piston displacement 236 cubic inches. N.A.C.C. rating is 28.36 h.p., maximum torque 190 lbs. ft. and actual horsepower 91 at 3200 r.p.m.

Heavy $2\frac{1}{2}$ " crankshaft is carried in seven large main bearings and counterbalanced for elimination of vibration and reduced bearing loads. Main bearings have a total area of 79 sq. in., and all bearings are of the precision type. Crankshaft journals are electrically surface-hardened by the "Tocco" process. Force-feed lubrication is employed with all passages drilled in the block. Water jackets extend the full length of cylinders and completely surround the exhaust valves and guides. Camshaft and water pump are driven by wide-faced steel front end gears carried in extra large bearings.

Lightweight pistons carry 4 rings each. Exceptionally large water pump is located at the right side of the motor. Combustion chamber is of the modified "high-turbulence" design, designed to secure maximum economy from standard grades of gasoline. Extraordinary efficiency of carburetion and manifolding results in unusually clean combustion and exceptionally low consumption of fuel. Special recirculating type full by-pass thermostat built in as standard equipment.

Engine cushion-cradled in a 4-point rubber mounting of new design which absorbs vibration almost completely. All parts designed for ease of inspection and ready servicing.

ELECTRICAL SYSTEM: Six-volt system, with heavy-duty 35-ampere generator, belt driven.

FRAME: A new type of X-frame especially developed to provide a solid foundation of extreme rigidity. Enormous strength and stiffness result from the use of heavy 6" side-rails reinforced by straight X-section of $8\frac{1}{2}$ " maximum depth, and 3 additional cross members, including wide steel tank support of special design. Front cross member extra wide and deep for positive protection from "weaving" at this point. X-member located to provide greatest reinforcement at rear of front springs and front of rear springs where greatest stresses are concentrated. Kick-up type for low loaded position—only 22 inches to top of frame at rear, with standard tires.

IGNITION: Distributor type.

GASOLINE CAPACITY: 18 gallons. Large tank at rear of chassis, die-formed and welded into one piece.

GROSS RATING: Maximum, including chassis, cab, body and load, 8,000 lbs. Chassis weight 2900 lbs.

LUBRICATION: Chassis lubrication by the Zerk system. No lubrication ever required for spring shackles, clutch throw-out bearing or braking system.

PAINT: Wheels and sheet metal Diamond T red, frame and running gear black. Sheet metal finished in enduring oven-baked enamel.

PROPELLER SHAFT: Balanced shaft with needle bearing universal joints.

PICK-UP BODY: (Special Equipment) The Diamond T eight-foot all-steel pick-up body, built throughout of 13-gauge and 14-gauge steel welded. Inside dimensions are 96"x46", with 17" sides to top of flares. Tailgate and vertical section of sides are 14" in height. Long running boards and rear fenders are standard equipment with this body, and all parts are finished in oven-baked enamel Diamond T red. Body weight approximately 500 lbs. Available only with single rear tires.

RADIATOR: Flat tube and fin core of special design for efficient cooling. Streamline radiator shell.

SPRINGS: Semi-elliptic, exceptionally long and wide. Front springs are 42"x2" and rear springs, 54"x2". All springs of silico-manganese steel. All spring shackles bushed with live rubber under compression, so that no lubrication is required for them for the entire life of the truck.

STEERING: Cam and lever, with latest twin-stud cam-follower and ball bearings in steering head.

TIRES: 6.00-16" 6-ply tires with special truck-type semi-drop-center rims of great strength are standard equipment. Options include 6.50-16" and 7.50-16" single tires and also 6.50-16" dual tires.

TRANSMISSION: Heavy duty type mounted in unit with engine. Four speeds forward and one reverse. All gears and shafts heat-treated alloy steel. Special full involute tooth contour of constant mesh and third speed gears promote long life without excessive wear. Main and counter shafts carried in ball and roller bearings.

WHEELS: Metal Spoke.

WHEELBASE: 119" for 8 ft. bodies (52" CA). Diamond T all-steel pick-up body is available.

DIAMOND T MOTOR CAR COMPANY — CHICAGO

(All Specifications Subject to Change Without Notice)

**It makes a pow
no matter what's**



erful statement under the hood.

If you're going to run a big cube engine, you better have room for it. That's the whole idea behind Mack Super-Liner®. You get all the power you want. Neatly packaged into one of the most powerful profiles on the highway. It's one handsome brute.

Strength in All the Right Places

Pick the power plant you want. Cummins. Caterpillar. Detroit Diesel or Mack Econodyne®. The long nose and unique, drop-front frame of Mack Super-Liner are designed to handle them and their radiators, with room to spare.

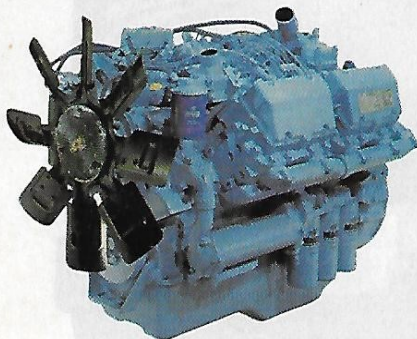
Three frames are available — one aluminum and two alloy steel. All are designed to distribute stress more evenly, improve stability and increase payload potential.

Mack Trucks' rugged dependability extends even to the cab. Its double-walled, galvanized steel body shrugs off rust and corrosion. And insulates you from noise and weather.

The View From the Top

Inside, the plush cab gives you a commanding view. Sit back in the standard air suspension seat. The wheel telescopes

into place. Check the individual gauges and controls on the wraparound console. Relax in any one of the spacious,



Mack Econodyne V-8



Detroit Diesel Silver 92

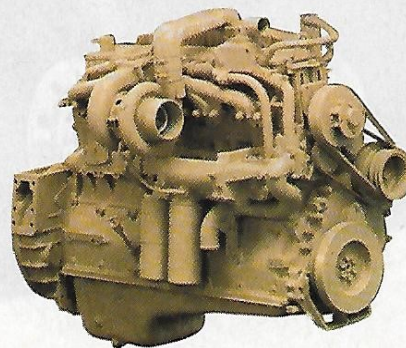
optional sleeper boxes. This truck does everything in its power to keep you on the road. And earning a profit.

And to keep you looking good, Mack offers a wide variety of options. Including chrome stacks and polished tanks. Special trim, hubs, mirrors and paint schemes. Everything

you need to make Super-Liner as individual as you are.

"Built Like a Mack Truck"®

Super-Liner is built to take on the long haul with confidence. With all the quality and inherent reliability Mack trucks are fa-



Cummins NTC-350

mous for. If you've got big ideas about trucks, you've come to the right place. Mack Super-Liner. The name on the hood makes a powerful statement about the man behind the wheel.



What America needs is less bull and more Bulldog.

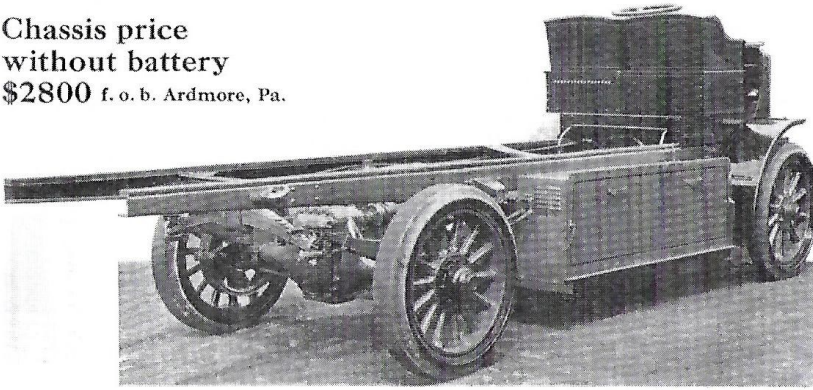
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CIRCLE 27 ON REPLY CARD FOR MORE DATA

BREAKING NEWS 1923

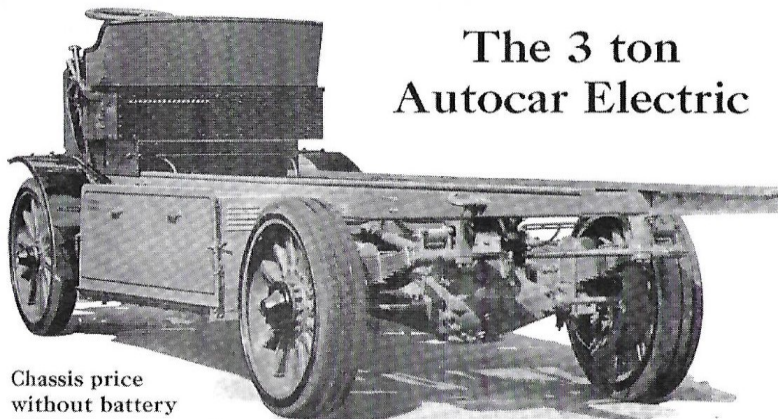
The 2 ton Autocar Electric

Chassis price
without battery
\$2800 f. o. b. Ardmore, Pa.



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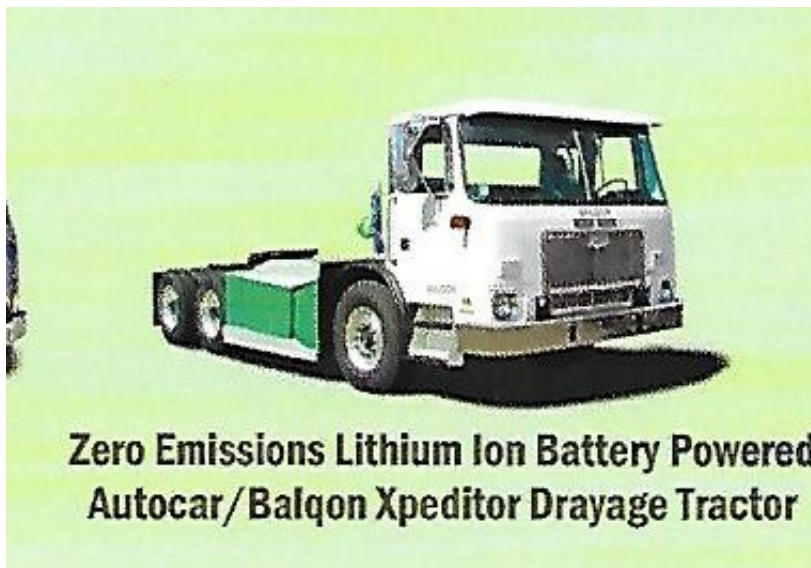
The 3 ton Autocar Electric



Chassis price
without battery
\$3200 f. o. b. Ardmore, Pa.

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BREAKING NEWS 2020



**Zero Emissions Lithium Ion Battery Powered
Autocar/Balqon Xpeditor Drayage Tractor**

IF PART OR ALL OF YOUR FLEET HAS THESE OPERATING REQUIREMENTS

**UP TO 10,000 GVW
UP TO 1 TON PAYLOAD
UP TO 60 MI. PER DAY
UP TO 50 MPH AT 35 MPH AVERAGE
150 TO 200 STOPS PER DAY**

THINK ELECTRIC NOW!



**THE U.S. POSTAL SERVICE ALREADY HAS OVER 330
SUCCESSFUL ELECTRIC VEHICLES ON THE ROAD,**
with another 50 about to go into operation.

We don't make electric vehicles but we are a major supplier of lead for their batteries. Write to us for more information about electric vehicles including a list of the companies that are making them right now.

PB-804

ST. JOE
MINERALS CORPORATION

250 Park Avenue, New York, New York 10017, Tel. (212) 953-5107



LEADING THE CHARGE



Electric meets Class. The Model 220EV blends the efficiency and maneuverability of the cabover configuration with a zero emissions, low maintenance, fully electric powertrain. The latest-generation batteries and power management system combine to deliver up to 200 miles of quiet performance. The Model 220EV is the perfect solution for pickup and delivery in urban environments, and is now available for order at Peterbilt dealerships coast-to-coast.



For more information on Peterbilt Electric Vehicles, visit Peterbilt.com.

CLASS PAYS

Editor's note on Electric Trucks:

I think I have the same feelings as many of our members about electric trucks. Guys in our age group would say, if it does not smoke, smell or make noise and have some kind of shifter, it isn't a real truck. The new trucks do not smoke anymore and they do not smell. The engineers were successful at this and they are more fuel-efficient. This came at additional cost and less reliability. The shifter is just about extinct now. It is very hard to find a driver with those skills these days. All we have left is the noise and if we lose that, there will be no listening to a skilled driver going through the gears leaving a truck stop or going up a hill. On the other hand, us real truck drivers that drove those real trucks all have hearing losses now, so maybe it will not bother us too much. Anyway, that is how I feel on this subject and being the Editor I guess I get to say how I feel.....

Brand New Sixty Years Ago

CHEVROLET TRUCKS

1961

*Pickups,
Chassis-Cabs
and Stakes—
Series C10, C20,
C30, C40*



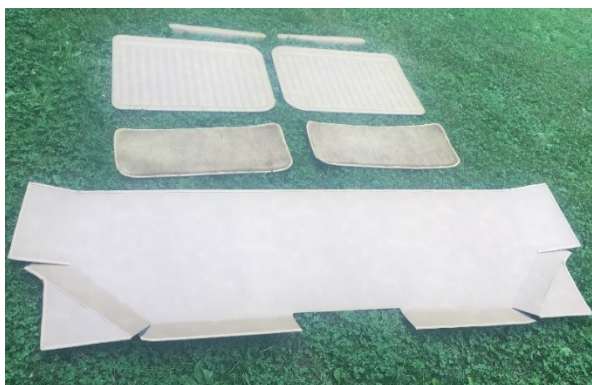
CHEVROLET TRUCKS

1961

worth more because they work more!



NUTMEG FOR SALE & WANTED
FOR SALE



Mack R/U/DM Models New Reproduced Tan Interior.
Includes 2 Door Panels with Carpet Bottoms and Arm Rests and Cardboard Panel for Behind the Seats.
\$1200.00 Call Jon at 860-709-3322 for details.

FOR SALE



VINTAGE MACK B & R Models 4" Chrome Exhaust Pipes
3' Tall with Bell. Brand New Never Been on a Truck.
A few available **\$350 each** includes shipping in USA. Call Jon at 860-709-3322

FOR SALE



18 FT 16 Cubic Yard Dump Body – No Hoist cylinder, No Cables or Brackets for Lift Arms. Set up for Exhaust to Heat Body. \$3,000. Located in Watertown, CT - Contact Lynn Dayton at 860-274-2998

FOR SALE



(Representative Photo)

RARE FIND – Components for World War I Vintage FWD Army Truck. Chassis with Four Good Wheels, Front & Rear Axles, and Frame in good shape. Left over from restoration project. Hard to find - \$750. Located in Roxbury, CT. Contact Dudley at 860-354-0649.

FOR SALE

1964 CHEVY C-50 DUMP TRUCK, 283 V-8 with 4-Speed Trans and 2-Speed Rear End. Needs restoration.
1965 CHEVY C-60 DUMP TRUCK, 283 V-8 with 4-Speed Trans and 2-Speed Rear End. Needs restoration.
\$300. for the pair. Contact Ray at 860-712-4602.

FOR SALE

1973 FORD F-700 DUMP TRUCK, V-8 with 5-Speed Trans and 2-Speed Rear End. Needs restoration. **Reduced to \$300.** Contact Ray at 860-712-4602.

FOR SALE

1942-1945 WILLY JEEP PARTS. Fenders, Hoods, Windshield, Radiator and some Engine Parts. Located in Chaplin, CT. Call Lee Johnson at 860-617-6445

WANTED

Left Side Door for 1986 AUTOCAR DK64FT Call Lance Violette 680-676-0411- Farmington, CT

WANTED: LEFT-SIDE WOODEN DOOR FRAME for a 1946 Autocar. Please contact Tom Holcombe @ 860-673-1936

WANTED:

COMMERCIAL CAR JOURNALS FROM 1945 to 1957
Will Buy or Trade Duplicate Copies for Same Years.
John Montville at (845) 462-6469 Noon & Evenings

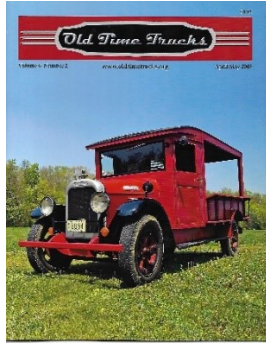
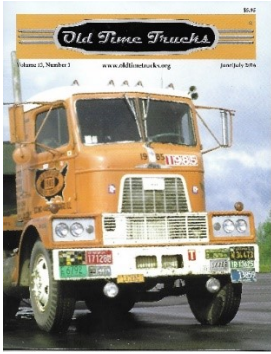


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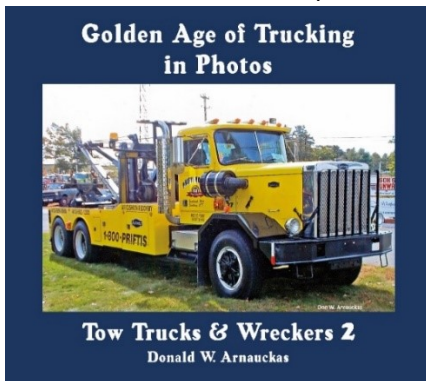


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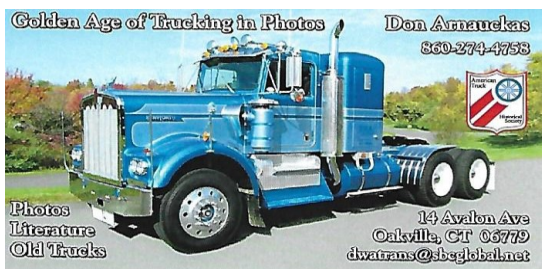
OLD TIME TRUCKS



Golden Age of Trucking Publishing purchased the Last Issue of "Old Time Trucks." This June-July 2016 issue was never mailed to subscribers. A limited supply of the May-June 2009 issue is also available. Supply is limited & will be mailed on a first-come-first served basis. Each Issue is \$6.00 plus \$3.00 postage. Send Check or Money Order to Golden Age of Trucking Publishing, 14 Avalon Avenue, Oakville, CT 06779. Call 860-274-4758 for more info & availability.



TOW TRUCKS & WRECKERS 2 is the latest in the series of Golden Age of Trucking Photos by Don Arnauckas, featuring 96 Photos of Tow Trucks & Wreckers from the 1930s-1990s from all over the U.S. Send Check or Money Order for **\$35 plus \$7** postage for each book ordered to: Golden Age of Trucking Publishing, 14 Avalon Ave., Oakville, CT 06779-2002. Other books in the Golden Age of Trucking in Photos series are also available. Call Don at 860-274-4758 or dwatrans@sbcglobal.net



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FROM THE EDITOR'S LOG BOOK

CARES & CONCERNS

- * **Dorothy Yeomans** fell inside her home recently and suffered a broken leg. She had surgery to repair her leg and will start rehab to get back on her feet hopefully in 6 weeks. Sending healing wishes her way.
- * **Cynthia Foskett** is on the road to recovery from a broken shoulder and is working from home these days. We wish her well in her recovery.
- * **Harold Foskett's** knee replacement surgery was postponed due to a spike in Covid-19. We will keep you posted on when he gets the green light.
- * **Tom Bachand** continues to do well following his back surgery. Happy to hear that his recovery is going well.
- * **Denis Yaworski** is recovering from eye surgery and is anxious for complete recovery.
- * **To our members** who have been affected by Covid-19 either by the illness itself or have lost family or friends, you continue to be in our thoughts and prayers as we share in this journey together. Please notify Don if you have anything that should be included in Cares & Concerns. Stay Connected, Stay Safe, Stay Well.

A member suggested recently that many of us may not own an antique truck, but are interested in trucking in general, its history, perhaps collecting photos, literature and trucking memorabilia. It is a very good suggestion and you will note that our application for membership now includes space for your trucking interests as well as any trucks you may own.

WELCOME to our newest NUTMEG MEMBER **Robert (Bob) Mansfield of North Branford, CT**

The Nutmeg Chapter continues to **UPDATE** our Roster of Members with current information. Therefore, **we request that you complete the Application below and return it with your payment of \$15.** This will enable us to keep our members informed via mail, email or phone of any important information such as cancelling or re-scheduling our events. This is especially important in these difficult times. We DO NOT share email information. Thank you in advance for your help!

Nutmeg Chapter of the American Truck Historical Society Membership Application

*****2021 MEMBERSHIP DUES are payable NOW *****

\$15 per Year - Mail to Membership Chairperson, Lance Dimock, Nutmeg Chapter-ATHS,
99 Notch Road, Bolton, CT 06043 Phone: 860-643-5224 or E-mail dimolalo@sbcglobal.net

Make check or money order payable to "Nutmeg Chapter ATHS"

IS THIS A NEW MEMBERSHIP _____ RENEWAL _____

NAME _____

SPOUSE'S NAME _____

MAILING ADDRESS _____

CITY _____ State _____ Zip/Postal Code _____

Email _____ (Used Only For Important Notifications & Newsletter, if requested.)

Phone Number (____) _____ Cell Phone (____) _____ No phone calls, just Newsletter _____

In what form do you wish to receive The Steering Column? Hard Copy _____ E-Mail _____ Both _____

Are you a Veteran? Yes ___ No ___ What Branch of Military? _____

IF YOU OWN A TRUCK OR HAVE A SPECIFIC INTEREST IN TRUCKS & THE INDUSTRY, PLEASE ENTER INFORMATION BELOW

MAKE _____ MODEL _____ YEAR _____

NUTMEG CHAPTER OF THE AMERICAN TRUCK HISTORICAL ESTABLISHED IN 1983 PUBLISHES THIS NEWSLETTER. EMAIL: dwatrans@sbcglobal.net
ATHS BYLAWS STATE THAT LOCAL MEMBERS MUST ALSO BE A NATIONAL MEMBER IN GOOD STANDING.